

East Maple Ridge Development Guidebook



April 2009 Final Draft

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Preface

This Development Guidebook for East Maple Ridge has been prepared for the Planned Unit Development, Preliminary Plat, and Major Development Review approvals from the City of Blaine, with the goal of responding to the letter to the applicant from the Planning Commission dated April 4, 2007. This Guidebook is composed in large part by materials that were included in the original application package considered by the Planning Commission in 2007, which have now been reorganized into the Guidebook format. Additional information has been included in this Guidebook to address the specific information requested in the April 4, 2007 letter.

The goal of this Guidebook is to provide a framework for review of the project that will comprehensively present the various features and components of the plan in an integrated format. Following approval of the project, this Guidebook will serve as a tool for developers, builders, and the City that will facilitate implementation of the planned project.

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1 Introduction

This Development Guidebook for the East Maple Ridge Plat Planned Unit Development and Preliminary Plat Applications has been prepared to organize and present information applicable to the project for review by the City of Blaine. It is expected that the specific design features, mitigation measures, and development standards in this document will be implemented as a condition of approval of the project. General descriptions of the site conditions and development proposal are intended to serve as background information for the City's review of the proposal. Upon approval of the project, this report is expected to serve as a guide for builders and City staff to implement the project standards and ensure that the development when constructed meets the intent of the proposal and the City's conditioned approval.

The following sections contain both specific descriptions of design constraints and features and general information on the various aspects of the site addressing the criteria for approval per City code.



Figure 1 – Site Aerial Vicinity Map

2 Project Summary

2.1 PROJECT DESCRIPTION

East Maple Ridge is a planned residential and mixed-use subdivision, developed as a Planned Unit Development PUD located in East Blaine. The project is consistent with the current zoning; Planned Residential PR. The 88.45-acre project will include a total of 353 dwelling units, including 128 units as single-family attached/multi-family and 225 units as single-family detached dwellings with an overall density of 4 units per acre. Multifamily housing includes a diversity of styles and ownership; Cottage Single Family Homes, Zero Lot Line Duplex, Row Houses, and Stacked Flats. To accommodate the Row Houses, this plat proposes an alternate plan to allow for fee simple ownership. A neighborhood commercial/civic center will accommodate mixed use and live/work opportunities as well as providing a focal point and meeting place for the community. The finished development will include approximately 24-acres of active and passive open space areas, including natural wetlands, created ponds and wetland features, natural woods, an active trail system, as well as developed common open spaces for park and recreational uses.

The project will be characterized by a friendly neighborhood feel with homes designed with a Northwest Craftsman style, as defined in the Architectural Guidelines.

Principle design parameters for East Maple Ridge include:



- *Reduction of the impact of cars and parking on the visible landscape by rear-loading garages with alley access to a majority of the units.*



- *Integration of several housing types into attractive neighborhood clusters.*



- *Preservation and integration of wetlands into the development.*



- *Incorporation of drainage ponds in the development as an attractive feature of the landscape.*

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- *Creating a network of pedestrian connectivity with sidewalks and trails.*



Figure 2 – PUD Site Plan

2.2 SITE LOCATION

2.2.1 Description of Location

The East Maple Ridge project area is located within the City of Blaine in Section 32, Township 41, Range 1E, W.M., as shown in the following figure. The overall project site contains approximately 88.4-acres of currently undeveloped forested land.

East Maple Ridge is located near the west end of the East Blaine City sub-area, annexed into the City of Blaine in 1996. The project is adjacent to existing residential housing, and City utilities and street access are available adjacent or near to the site. Because this location is in proximity to existing services, this project does not require extensive development of City infrastructure.

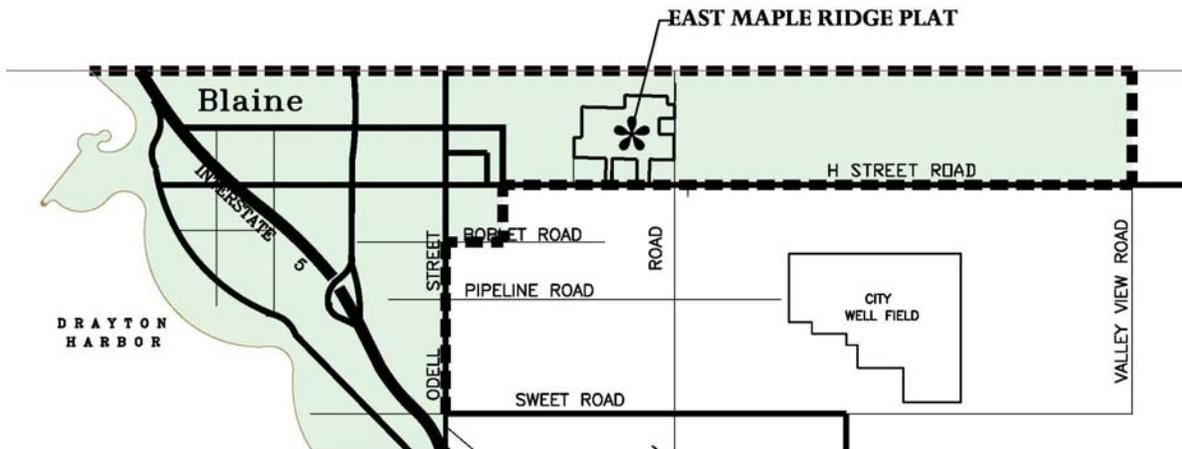


Figure 3 - Vicinity Map

2.2.2 Zoning

The project site area is zoned Planned Residential, with the applicable City code section being BMC Chapter 17.42. This zone allows a density of up to 4 dwelling units (DU) per acre, if City standards for street and utility servicing are met, and if the site is approved as a Planned Unit Development per BMC Chapter 17.48. If these conditions are not met, the zoning allows only 1 DU per 5 acres.

2.3 PROJECT QUANTITATIVE DATA

Total number and type of residential units:

188 single-family

37 single-family cottage

128 multi-family / single family townhouse

353 total residential units

Parcel Size: 88.45 acres

Proposed lot coverage of buildings and structures: 35%

Approximate gross residential density: 4 DU/Acre

Approximate net residential density: 4 DU/Acre

Total amount of open space: 23.6 Acres (20%)

Amount of usable open space: 9.97 Acres

Total non-residential construction: 24,000 sf

2.4 SUMMARY OF PERMIT REQUIREMENTS

2.4.1 Preliminary Plat

Subdivision of the property into parcels for development of homes requires approval of a preliminary plat prior to development, in accordance with the Blaine Municipal Code. Final plat approval for each phase will follow completion of construction or bonding for required infrastructure.

2.4.2 Planned Unit Development

The site zoning requires the approval of a Planned Unit Development District per BMC Chapter 17.48, to allow the development density proposed for this application (greater than 1 unit per 5 acres).

2.4.3 Major Development Permit

This project requires a Major Development Permit review, which means that the proposal must be considered by the City Council for approval.

3 Compatibility with Existing and Future Development

3.1 PROJECT INTENTS AND GOALS

East Maple Ridge will provide quality residential housing for the City of Blaine in an integrated development that will protect the value and use of neighboring property, provide large open space areas, retain wetlands with buffers in a natural state, and provide amenities for the development and the neighborhood including recreational space, trails, street connections, and a small commercial center.

This development will help achieve the long-term goals and objectives of the City in East Blaine by complying with requirements and intent of the City Code and the Comprehensive Plan. The current City of Blaine Comprehensive Plan, as amended in 1999, planned for a projected development of 500 to 1000 new lots and 500 new homes on new and existing lots in the East Blaine area by 2015. Over the course of its phased development and build-out, East Maple Ridge will contribute substantially to providing housing to meet those goals.

3.2 COMPATIBILITY WITH CITY CODE PURPOSE STATEMENTS

3.2.1 BMC Chapter 17.42 Planned Residential District

BMC 17.42.010 Planned Residential Zone, Purpose

“The intent of the residential zone is to promote an orderly transition from a rural to residential development, to encourage land uses and associated densities which will be complementary with existing rural densities, while allowing reasonable transition uses of the properties...”

The proposed project incorporates the densities allowed under the PR zone ordinance, providing for transition to the higher clustered lot densities by providing open space and landscaped stormwater features, and perimeter buffers that will preserve existing mature vegetation wherever possible.

3.2.2 BMC Chapter 17.48 Planned Unit Development

BMC 17.48.010 Planned Unit Development District, Purpose

“To permit greater flexibility and consequently more creative and imaginative design for the development of residential and business uses than is generally possible under conventional zoning regulations...To promote more economical and efficient use of the land while providing a harmonious variety of housing choices, a higher level of city attractiveness and quality, and preservation of scenic open space...”

The proposed project has implemented a creative design that has addressed the concerns and objectives suggested by the City in the two year review process that this project went through prior to the public hearing. The flexibility provided in the PUD code has allowed the provision of a range of lot sizes to accommodate a diverse market, the preservation of on-site wetlands and open space areas, the implementation of LID principles for stormwater management by reducing impervious surfaces, and the reduction of the impact of car parking and traffic on the character of the project through the use of alley-driven lots.

3.2.3 BMC Chapter 17.56 Preliminary Plat

The Preliminary Plat code section BMC 17.56 does not contain a purpose statement, however the application and materials in this Guidebook are in compliance with the submittal standards in that code section.

3.3 COMPATIBILITY WITH CITY COMPREHENSIVE PLAN GOALS

3.3.1 Population Growth

The East Maple Ridge PUD proposes 353 total new residential dwelling units, with a projected population of 847 people, assuming 2.4 persons per dwelling unit. The 2002 Blaine Comprehensive Plan growth projections anticipated 250 new dwelling units in East Blaine by 2006, which have largely failed to be built. Development of this property (anticipated to begin in 2009) will help the City meet their growth projections in accordance with the Comprehensive Plan.

3.3.2 Housing

Goal 1: "To encourage the development of a variety of housing types and prices, including an adequate supply of housing in a price range affordable to employees at available jobs in Blaine and housing which meets the needs of senior citizens."

East Maple Ridge will include a variety of housing types including traditional single-family detached, single-family attached, detached cottage units, row houses, and multi-family residential buildings.

Goal 7: "To encourage the development of affordable housing within the City without sacrificing public safety or the ability to provide needed public services and utilities."

It is expected that East Maple Ridge will provide a range of affordable neighborhood housing.

3.3.3 Economic Development

Construction and development of East Maple Ridge will support the local construction sector, and the new population will increase the local market for goods and services, encouraging growth of commercial services and other economic development.

For information on likely economic affects of the project, see the Fiscal Impact Study prepared by Northwest Economics, which says...

3.3.4 Land Use

GOAL 4: "To protect the scenic beauty, water quality, wildlife habitat areas, open spaces and cultural resources which contribute to the quality of life and give the Blaine area a rural character."

East Maple Ridge will preserve the small area of wetlands located on site, as well as incorporating approximately 24-acres of open space into the development, including integration of the proposed stormwater quality ponds into passive features of the usable open space areas.

3.3.5 Transportation

GOAL 1: "Promote convenient, accessible, safe and environmentally responsible multi-modal transportation for residents, employers and employees, visitors and commerce."

The proposed site plan and street sections include provision of multi-use trails and sidewalk connections for pedestrians along the streets, and through the development. The project will provide a significant link in the City's planned multi-modal transportation link along Motts Hill Parkway, which has been incorporated into the development plan.

3.3.6 Utilities

East Maple Ridge will provide a crucial link to the extension of utilities and development of East Blaine, as the first large property to be developed, located in the western end of the sub-area in proximity to City utilities. The project planning effort to extend utilities to this site has been underway with the Public Works Department since before 1996, and is currently planned to proceed in general accordance with the City's Sub-Area Infrastructure Plan, currently in progress by the Public Works and Community Development departments.

3.4 COMPATIBILITY WITH EXISTING LAND USE

3.4.1 Density

The surrounding adjacent residential properties are currently significantly less dense than the proposed development, as is all of the East Blaine Sub-Area currently. East Maple Ridge will protect neighboring properties and help provide compatibility by:

- (1) Maintaining a 30-foot natural vegetated buffer around the perimeter of the project where abutting lower-density residential properties.
- (2) Locating stormwater quality ponds and open space areas where possible on the perimeter of the property, to provide additional buffering to neighboring residences.

3.4.2 Circulation Systems

The proposed street circulation system has been designed to promote compatibility with the surrounding properties by locating proposed street connections to both facilitate transportation connections where needed by neighboring properties, and to limit impacts from traffic on the existing neighborhoods. This is reflected in the site plan by the provision of a connection to the Caples Addition neighborhood, to Motts Hill Parkway, and by *not* proposing a street connection to Harvey Road, other than Motts Hill Parkway in accordance with the City's infrastructure plan.

3.4.3 Public Facilities

It is not anticipated that provision of public facilities to the project will have any effect on the developments compatibility with existing land uses in the project vicinity (as discussed in the Transportation and Utility sections, below). City water is currently available on the property, and extension of sewer service is planned.

3.4.4 Natural Features

The project has been designed to be compatible with the natural features of the property by:

- (1) Preserving existing vegetation in open space, where possible.
- (2) Preserving and incorporating existing wetland areas into the project open space.
- (3) Providing open stormwater ponds to maintain the existing drainage pattern and flow rates.

3.5 COMPATIBILITY WITH LIKELY FUTURE DEVELOPMENT

It is expected that future development in the surrounding vicinity will be of a similar character and density as East Maple Ridge, as the surrounding area in the City is also zoned Planned Residential.

The project layout and plans for streets and utilities have been designed in close cooperation with the City Public Works and Community Development departments, to harmonize with the ongoing effort to comprehensively plan for the development of infrastructure to serve East Blaine. The site plan's incorporation of Motts Hill Parkway as a central feature underscores this effort.

Additionally, because of this site's unique location in the East Blaine Sub-area, with regard to availability to water, sewer, and street connections, East Maple Ridge is situated as a necessary first element for the further extensions of City services to promote the development of East Blaine and meet the City's Comprehensive Plan goals.

3.6 SITE DESIGN BENEFITS OF PUD PROCESS

Principle design parameters for East Maple Ridge, which have resulted as a benefit of the flexibility allowed in the PUD process, include:

- *Reduction of the impact of cars and parking on the visible landscape by rear-loading garages with alley access to many units.*
- *Integration of several housing types into attractive neighborhood clusters.*

- *Preservation and integration of natural wetlands into the development.*
- *Incorporation of drainage ponds as an attractive feature of the landscape.*
- *Creating a network of pedestrian connectivity, with sidewalks and foot trails.*

4 Site Information and Background

4.1 HISTORY OF PROPERTY

The subject property has been owned by Doug and Louise Connelly since the late 1980's. The site has historically been maintained as wooded land, and periodically logged for timber.

In 1996 the property was annexed into the City of Blaine, along with the rest of the East Blaine Sub-area. Planning efforts to develop the property into a residential neighborhood have been underway since at least 1993.

4.2 DEVELOPMENT AGREEMENT

When the East Blaine annexation was completed in 1996, a Development Agreement was put in place between the City of Blaine and property owners in the East Blaine annexation area. The Agreement affecting the East Maple Ridge property was amended in April 2005, following public notice and hearings.

The Amended Development Agreement stipulates obligations of both the City and the Developer. The Developer Obligations are, in summary:

- (a) Payment of a fee to help fund the City-developed Sub-Area Infrastructure Plan for East Blaine (obligation completed in 2005).
- (b) Payment of a one-time mitigation fee for each building permit, to support costs of police protection.
- (c) To protect aquifer recharge areas and wetlands.
- (d) To construct water, sewer, and street improvements as needed to serve the development, and where needed, sized to accommodate future development within East Blaine.

The City's obligation under the Agreement is to permit utilization of latecomer's agreements or LIDs for infrastructure where appropriate, and to cooperate in seeking matching funds, state, and federal grants that may provide additional funds to the City for construction of sewer, roads, water, open space, trails, parks, and related infrastructure within the East Blaine sub-area.

The Amended Development Agreement, as recorded under AF # 2050901889, is attached as Appendix G.

4.3 EXISTING SITE CONDITIONS

4.3.1 Topography

The site straddles a broad hilltop that divides the site with an east-west ridge running across the center of the site. The site slopes from a high elevation on the east side of approximately 340 feet to a low of approximately 260 feet on the southwest side.

4.3.2 Watercourses

There are no watercourses located on the East Maple Ridge site, however, the stormwater runoff from the site

4.3.3 Soil/geologic Conditions

The site soils are mapped within the Everett Gravelly Sandy Loam soil unit, and a portion of the southwestern site area is within the Sehome Gravelly Loam soil unit, #141 (USDA/SCS, 1992). The Everett soils are described as deep and somewhat excessively drained, and are categorized as hydrologic soil group 'B.' However, these soils also exhibit a perched groundwater table and hard glacial till soils at a depth of approximately 5-feet below ground surface, making this area generally unsuitable for infiltration of stormwater.

4.3.4 Unique Natural and Built Features

There is one existing residence on the property that is proposed to be removed. The site contains a 12-inch diameter City-owned water main that bisects the property from east to west.

Substantial additional information and descriptions of the site features and conditions can be found in the project SEPA checklist (Appendix A), the Existing Site Conditions Map (Section 12), the Preliminary Stormwater Site Plan (Appendix C), the Geotechnical Report (Appendix E), and the Wetland Delineation Report (Appendix B).

4.3.5 Forest Cover

The property is largely vegetated with existing forest cover, consisting of third growth Douglas Firs, Big Leaf Maples, Alders, and Western Red Cedars.

4.3.6 Wetlands

A critical areas analysis of the site was completed in July of 2004 by MRM Consulting (now Northwest Wetlands Consulting LLC). The delineation identified 0.40-acres meeting wetland criteria, in the southern area of the site, rated as Category III wetlands based on City of Blaine Criteria. The Wetland Delineation Report documenting the site wetland conditions and findings is attached as Appendix B.

The project does not propose any impacts to wetlands; the design preserves and incorporates the wetlands into the site open space plan.

4.3.7 Wildlife Corridors

The site does not contain any known wildlife corridors or significant habitat.

4.4 AQUIFER RECHARGE STUDY

A site geotechnical soils evaluation and aquifer recharge study was completed by Jim Bailey of Kleinfelder in December 2006, and the findings reported in a memorandum dated December 11, 2006 (attached as Appendix E). The subsurface investigation and analysis found that the soil conditions include surface layers of topsoil and outwash sand, underlaid by hardpan glacial till at a depth of 3 to 5-feet below the ground surface.

The study concluded and recommended that “...the site is not a significant source of direct recharge to beneficial aquifers located beneath the glacial till.”

4.5 SITE ACCESS AND TRAFFIC ANALYSIS

Primary access to the site is currently from H Street on the south, and from E Street on the west side of the property. A new access road from H Street will be constructed with the first phase, and this will serve as the principal site access point for Phases 1 – 3. Phase 4 will construct a segment of Mott’s Hill Parkway connecting to the existing E Street access point, which will be extended through to Harvey in accordance with the phasing plan (see Section 5, below). With eventual full construction by others of the Mott’s Hill Parkway link to downtown Blaine and SR-539, Mott’s Hill Parkway will become a principal access corridor to East Blaine, as described in the East Blaine Infrastructure Plan prepared by the City.

See Section 10.1 in this Guidebook for a description of the proposed off-site street improvements, and the Traffic Analysis Report prepared by Gibson Traffic Consultants (Appendix D) for more detailed information and analysis of the existing traffic and impacts from the development.

5 Project Phasing

5.1 OVERVIEW OF PHASING PLAN

East Maple Ridge is proposed to be developed in 13 phases, allowing the project to grow and be built flexibly over time. The phasing plan is based on building the site areas closer to utility and street connections first. With each phase, the developer must construct the streets and utilities needed to serve that phase, and provide for extensions into the future phases.

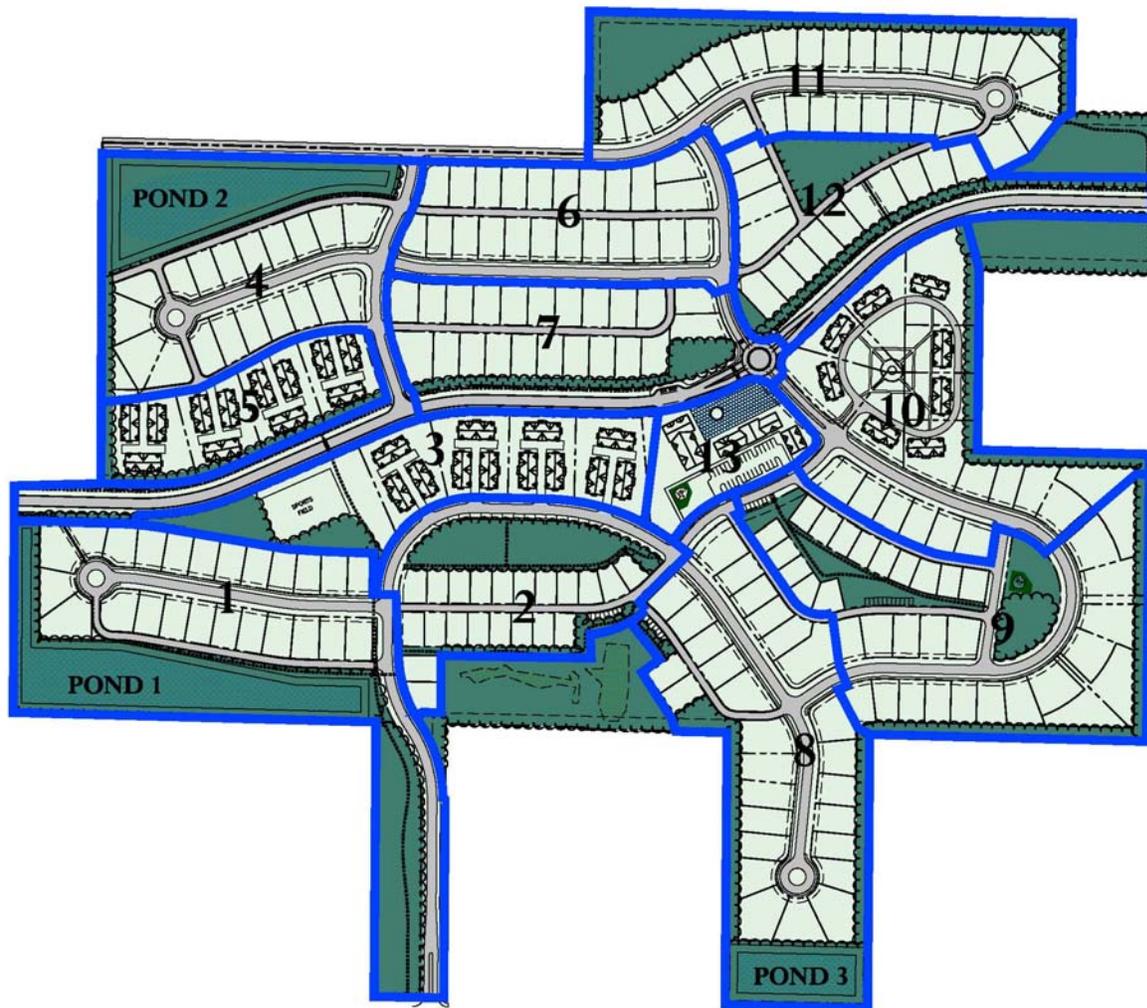


Figure 4 - Project Phasing Plan

While each phase, at a minimum, must include the construction of the on-site streets, utilities, trails, and other facilities needed within that phase, and for connections to existing improvements, several phases will require construction of critical on- or off-site infrastructure elements. These are listed in the following table.

Project Phase Data

<i>Phase</i>	<i>Dwelling Units</i>	<i>Type (SF/MF/COM)</i>	<i>Public Improvements</i>
1	24	SF	<ul style="list-style-type: none"> ▪ Sanitary sewer main extension from H Street ▪ Stormwater Pond 1
2	21	SF	
3	48	MF	
4	19	SF	<ul style="list-style-type: none"> ▪ Mott's Hill Parkway segment from E Street to serve phase ▪ Stormwater Pond 2
5	48	MF	
6	23	SF	<ul style="list-style-type: none"> ▪ Cedar View Street construction
7	22	SF	<ul style="list-style-type: none"> ▪ Mott's Hill Parkway extension to roundabout intersection
8	28	SF	<ul style="list-style-type: none"> ▪ Stormwater Pond 3
9	34	SF	
10	43	SF/MF	
11	29	SF	
12	14	SF	<ul style="list-style-type: none"> ▪ Mott's Hill Parkway extension to Harvey Road
13	0	COM	

5.2 SECURITY FOR IMPROVEMENTS

Security for the improvements needed for each phase will be provided by the standard public facilities extension performance bonding through the Public Works Department, prior to approval for construction. This is the City's

standard mechanism for ensuring that construction of public infrastructure is completed and meets City standards. Each phase will also be required to complete all necessary infrastructure to meet City standards prior to approval and filing of a Final Plat for that phase.

5.3 CONCURRENCY

Utility, transportation, storm drainage, and other servicing needs for East Maple Ridge have been analyzed to determine existing capacity, and to identify improvements needed to serve the development. Discussion of these elements is included in Section 9 of this Guidebook. The phased construction of critical elements in the servicing plan will provide adequate capacity to serve the development at each phase.

6 Land Use Development

6.1 LOT TYPES

A principal design parameter for East Maple Ridge is the integration of several housing types into attractive neighborhood clusters within the development. The project will incorporate opportunities for several housing types, as shown in the following figure, and discussed in more detail in the following sections.

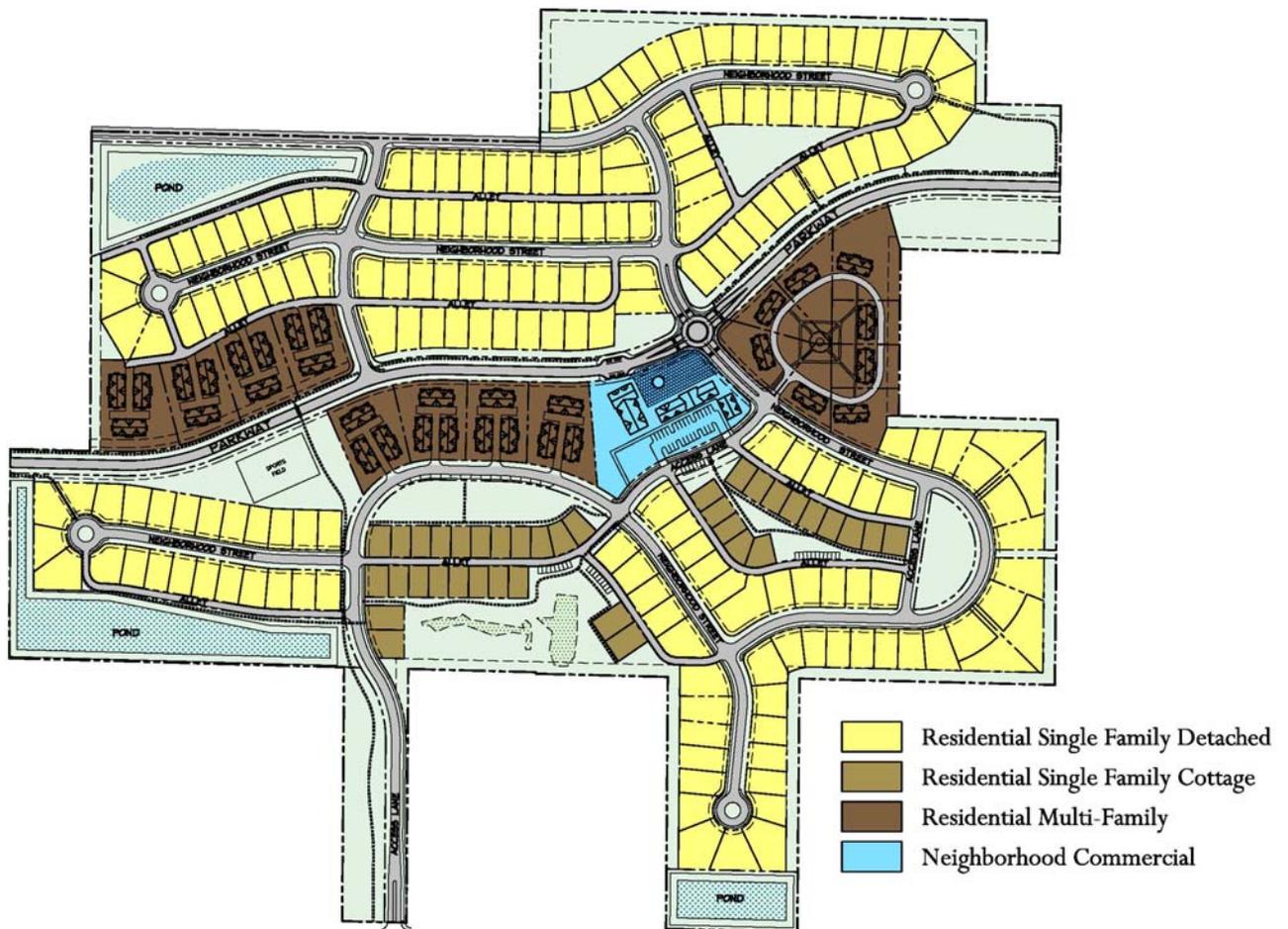


Figure 5 - Lot Types

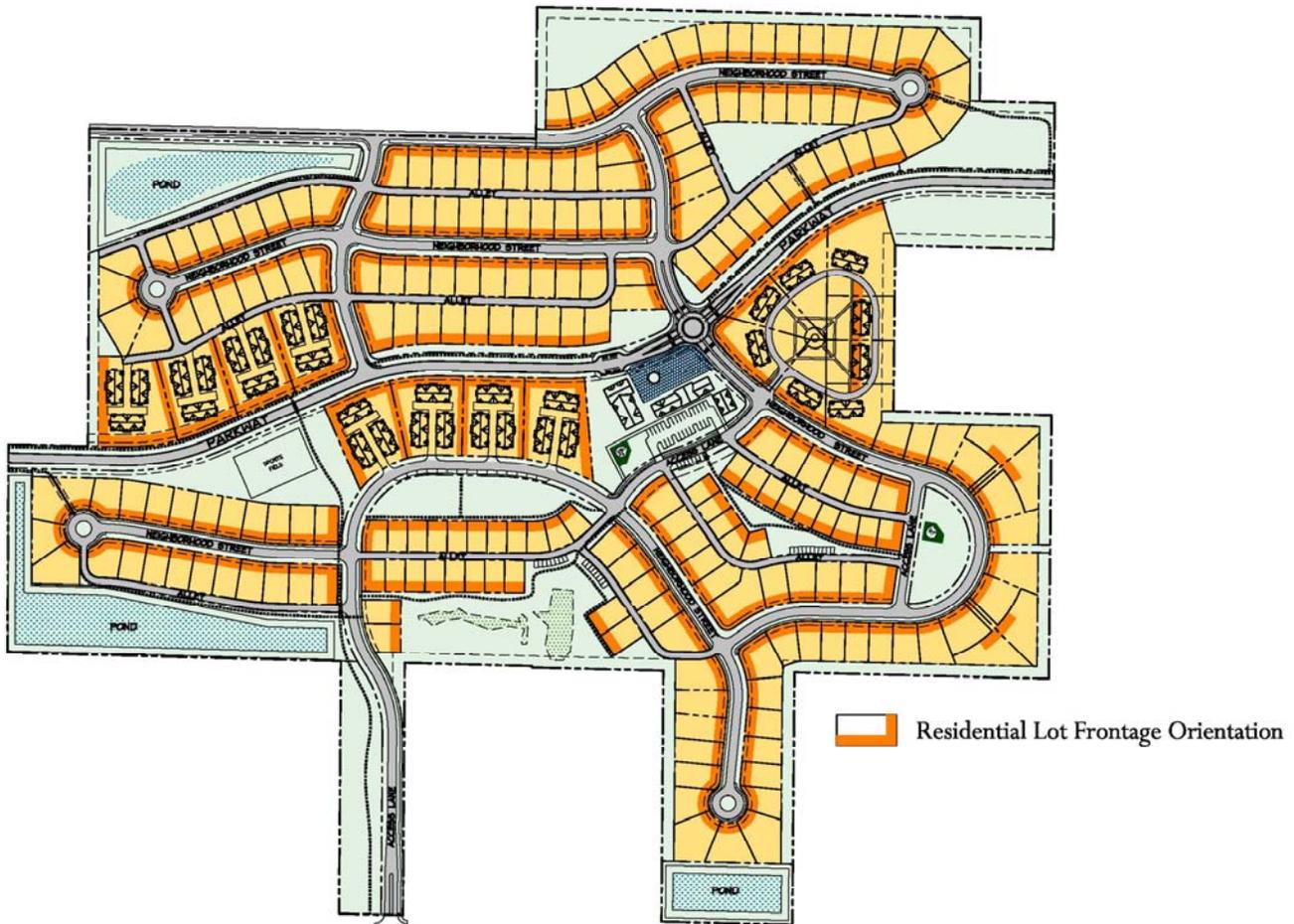


Figure 6 - Lot Frontage Plan

6.2 SINGLE-FAMILY DETACHED

Residential single-family detached lots in East Maple Ridge range in size from 4000 sf to 13150 sf, and will accommodate a wide variety of home sizes. As specified in the Design Standards (see Section 7), these lots must front on the principal adjacent street, and must access garages from an alley where possible.

Units designated as Single-family Detached may also include Single-family Attached units in accordance with BMC 17.31 and with specific review by the City and the Architectural Committee, provided that alley loaded garages are provided.

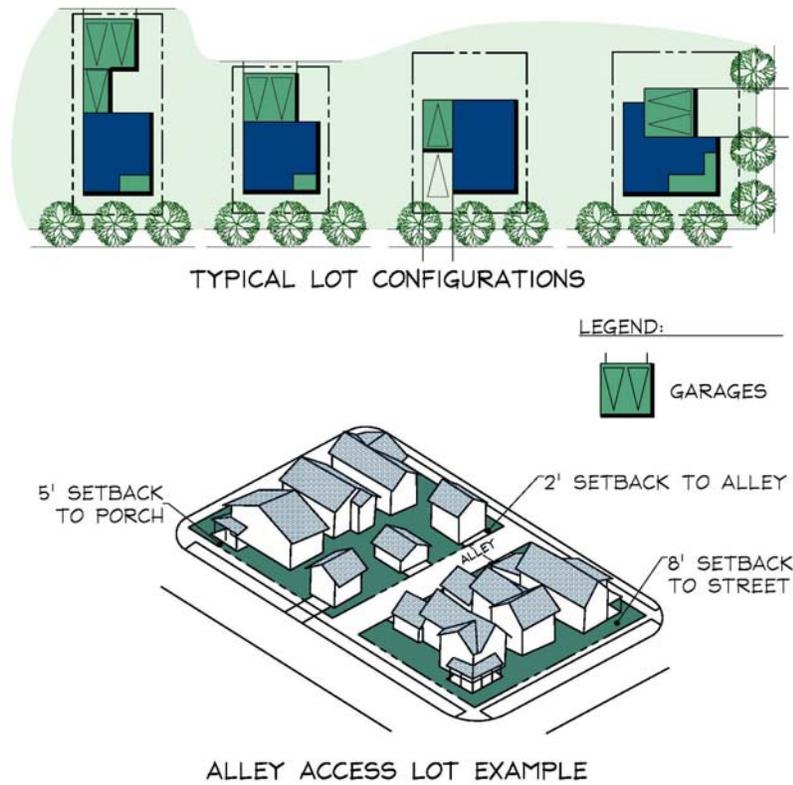


Figure 7 - Single Family Detached Example

6.3 COTTAGE HOUSING

Cottage housing consists of small, single-family, detached homes on lots sized 3000 – 4000 square feet. Vehicular access to these units will be from alleys, and shared garage structures will be provided wherever possible.

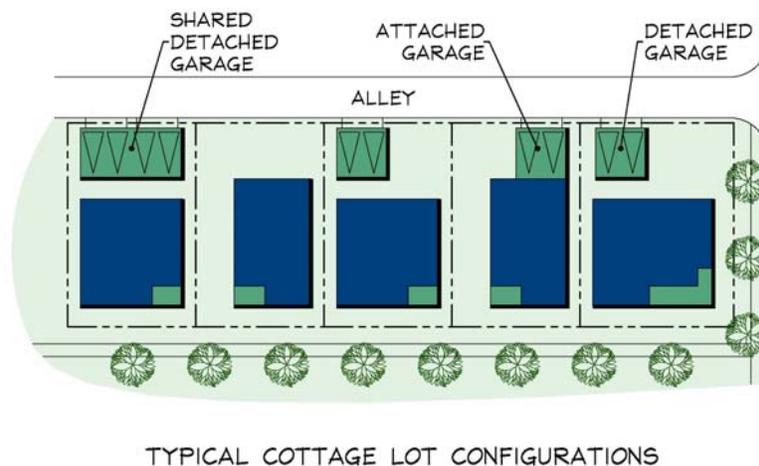


Figure 8 – Cottage Lot Example

6.4 MULTI-FAMILY

The designated Multi-family tracts on the PUD Site Plan will accommodate up to 128 dwelling units in buildings up to 45 feet in height. It is planned that these buildings will consist of 4-unit row houses with garage parking at the first level, served by common access driveways, and with the building “fronts” facing onto common open space and park areas.



Figure 9 - Multi-family Site Plan Example

All building proposals within the site multi-family tracts will require Site Plan Approval from the City of Blaine Community Development Department in accordance with BMC 17.07 prior to submitting building permits. Based on site plan approvals, the sites may be configured either as fee-simple Row-house lots, or as standard apartment or condominium units on a single tract.



Multi-family Garage Example



Common Garage Access Example

Alternative designs for multi-family residential building styles or designs may be considered for these tracts when: (1) The overall density of the tract or tracts is not increased; (2) The proposed building style and site design is compatible with the approved PUD and surrounding development; (3) The proposed site plan will benefit the development and provide better achievement of the design parameters, goals, and objectives of East Maple Ridge, as defined in this Guidebook; and (4) the proposed site plan must be specifically reviewed by the

City and the Architectural Committee for consistency with the intents and goals of this Guidebook.

Specific conceptual alternative designs that may be considered within the above criteria include the following options:

- Units may be designed as Stacked Flats or Row-houses with detached shared parking structures. Up to 10% of the units in each tract may be configured as carriage-house units over shared parking.



Stacked Flats Example – With Exterior Entry



Carriage House Example



Units Fronting on Common Greenspace



Shared Garage Access Example

6.5 NEIGHBORHOOD COMMERCIAL

The East Maple Ridge PUD plan includes a small neighborhood commercial site located roughly at the center of the project, at the intersection of Mott's Hill Parkway and the main development access road to the Parkway. The commercial site is planned to accommodate approximately 20,000-sf of building space, with common-use open plaza and an adjacent playground.

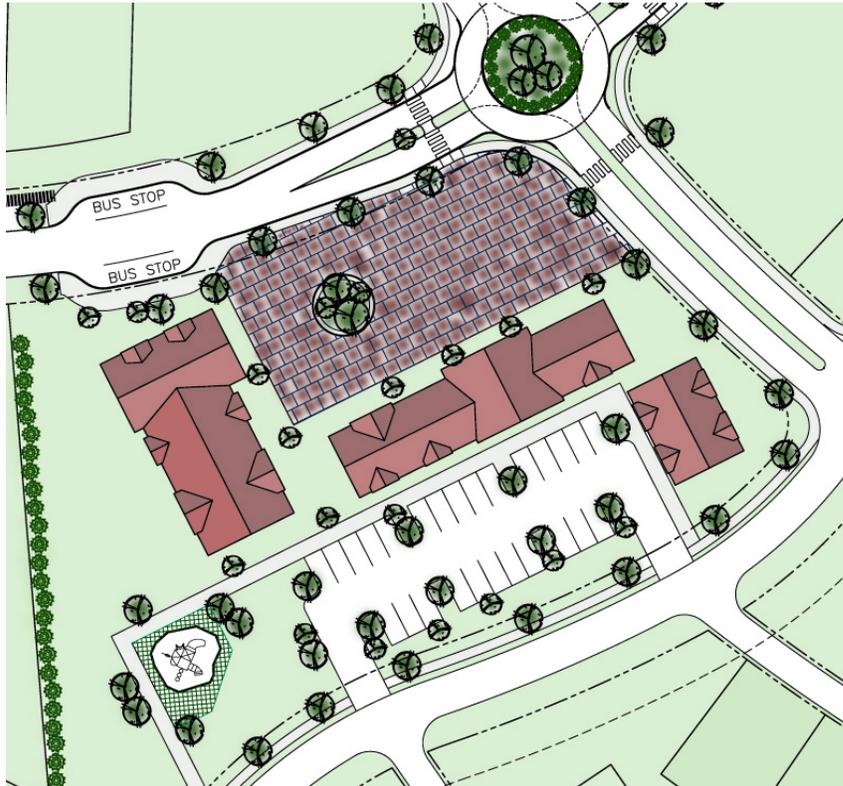


Figure 10 - Commercial Site Concept Plan

Building proposals for the commercial site will require Site Plan Approval from the City of Blaine Community Development Department in accordance with BMC 17.07 prior to submitting building permits. In addition, building proposals shall:

- Include public plaza.
- Include playground.
- Be compatible in design with adjacent multi-family residential units.

- Have a layout generally as illustrated in Figure 8.

7 Development Standards

7.1 LOT STANDARDS

These lot standards are intended to amplify the mutual relationship between housing units, roads, open space, and pedestrian amenities for creating small lot development that protects the privacy of individuals while creating pedestrian-oriented environments.

A. Lot Size.

1. *Design Objective.* Provide flexibility in lot sizes to enable creative pedestrian-oriented neighborhoods.

a. Standards.

(1) Lot Size. There is no minimum lot size.

b. Guidelines.

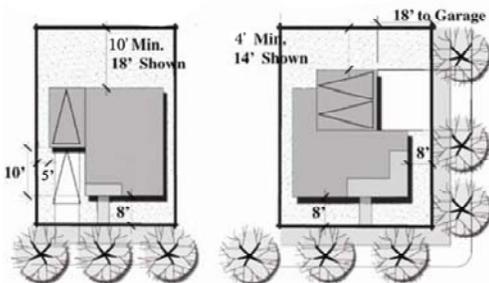
(1) Small lot development with numerous lots should incorporate a variety of home sizes and unit clusters to discourage monotonous neighborhoods.

B. Building Setbacks.

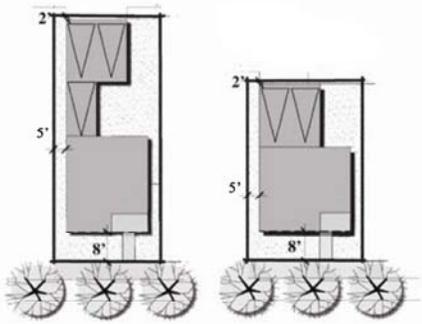
1. *Design Objective.* Setbacks shall ensure separation of homes and private spaces while allowing high density.

a. Standards.

(1) Front on Neighborhood Street: 8 feet to building, 5 feet to porch, 7 feet to stoop.

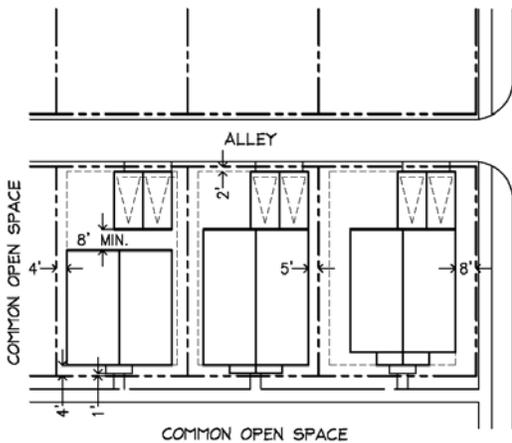


Building Setbacks for Lots Fronting
on Neighborhood Street



Building Setbacks for Lots Fronting on Neighborhood Street and Backing on Access Alley/Lane

- (2) No lot shall front on alleyways unless explicitly approved by city as a variance
- (3) Side: 5 feet.
- (4) Side on Corner Lot Facing Street: 8 feet.
- (5) Rear: 4 feet adjacent common open spaces, otherwise 10 feet.
- (6) Alleyway: 2 feet from alley right-of-way.
- (7) Front on a Pedestrian Easement or Common Open Space: 4 feet to building or 1 foot to porch or stoop.



Building Setbacks for Lots Fronting and/or Siding Pedestrian Easement or Common Open Space

- (8) Side on a Pedestrian Easement or Common Open Space: 4 feet.
- (9) Decks are to be considered part of the building and shall not intrude into setbacks. BMC code provisions for "Architectural Projections into Yards" shall not apply in East Maple Ridge.

b. Guidelines.

- (1) Visual and functional continuity should be maintained between housing units through similar setback, and/or landscape buffer.
- (2) Structures and parking areas may encroach into required setbacks if it can be shown that such encroachment allows significant trees or tree clusters to be retained. Encroachment shall be the

minimum encroachment necessary to protect specified trees. In no case shall the yard be reduced by 50 percent or more of the required setback.

C. Reciprocal Use Easements.

1. *Design Objective.* Allow opportunities to maximize space through the use of reciprocal use easements.

a. Standards.

(1) If used, reciprocal side and/or rear yard use easements shall be delineated on the site plan presented with the building permit application.

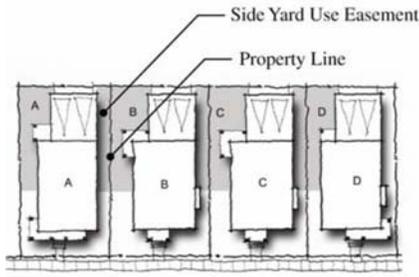
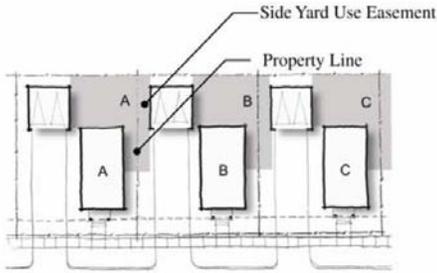
(2) If a side yard easement is used, the wall facing the side yard shall be constructed as a “privacy wall.” Privacy walls shall not have doors entering into the yard space of the adjacent home, nor have windows that are within 5 feet of ground level.

b. Guidelines.

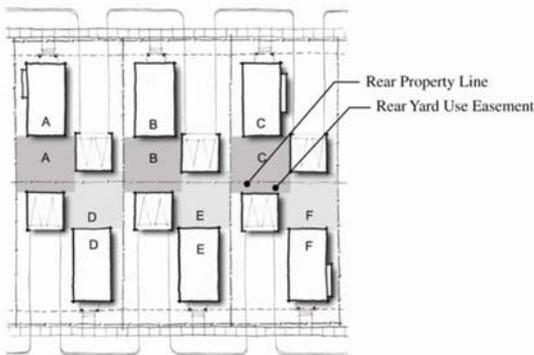
(1) The use of reciprocal side and/or rear yard easements is encouraged.

(2) The design of use easements should not negatively affect the building foundations.

(3) Given the intimate relationship between adjacent houses, it is extremely important to carefully lay out each home on its lot to maximize this outdoor space.



Side Yard Easement Variations



Rear Yard Easement

D. Parking Requirements.

1. Residential Single Family.

a. Standards.

(1) Required Parking Spaces.

(a) One garage stall per unit is required for units 1,500 sf or less. Two garage stalls per unit is required for units over 1,500 sf.

(b) Garage stalls shall be located on the lot or in a shared detached garage.

(2) Required Guest Parking.

(a) One guest stall per unit is required and may be located on the lot. Parking is not permitted on access lanes or alleyways.

(b) The tandem driveway space associated with on-site garages may be counted toward the guest parking requirement.

(3) Driveways.

(a) A tandem driveway space is allowed on a lot and shall be a minimum of 18 feet in length from back of sidewalk or back of access lane.

(b) The width of the driveway curb cut shall not exceed 9 feet for single lane and 16 feet for double lane. An individual driveway shall serve a maximum of four units.

b. Guidelines.

(1) Guest parking should be within an acceptable walking distance to the housing it is intended to serve, a maximum of 300 feet from the closest lot line

2. *Multi-Family.*

a. Standards.

(1) Parking standards for multi-family, including single-family attached units shall be in accordance with the City of Blaine Municipal Code.

b. Guidelines.

(1) Parking shall be located in a manner minimizing visibility from the street and is preferred in the following order:

- Under the building
 - Within an attached or detached garage
 - Off of an alley
 - Behind buildings
 - Screened from adjacent streets and public areas
-

3. *Commercial.*

a. Standards.

(1) Parking standards for commercial area shall be in accordance with the City of Blaine Municipal Code.

b. Guidelines.

(1) Parking must be located in a manner minimizing visibility from the street. For parking areas adjacent to street, parks or other public areas, the landscaping design shall include screening buffers or other design features to be specifically reviewed by the City of Blaine and the Architectural Committee.

(2) City of Blaine Municipal Code
“Landscaping “ regulations shall apply to commercial parking lot design.

7.2 ARCHITECTURAL GUIDELINES

A. Guidelines for Single Family Attached and Detached Housing.



Northwest Craftsman Example

1. *Overall Goals.* Single family homes are to be in the Northwest Craftsman style. Cottage Single Family Homes, Single Family Attached Homes, Row Houses, and Stacked Flats are also to be designed with the Northwest Craftsman style (see definitions).

2. *Architectural Diversity.* In order to ensure architectural variety provide the following:

a. For all single family detached and single family attached homes: No two houses having the same floor plan, frontage elevation, and roof plan may be built within 500 feet from each other as measured along the centerline of the fronting street by the shortest possible route.

b. For all single family detached and single family attached homes: No more than five houses may be built with the same floor plan within 500 feet from each other as measured along the centerline of the fronting street by the shortest possible route.

c. For all buildings: No two identical color schemes can be placed side by side or directly across the street from one another.



Example Illustrating Lack of Architectural Variety

d. Determination of acceptable architectural diversity in accordance with this section shall be the responsibility of the Architectural Review Committee, and subject to approval by the City of Blaine.



Row House Example

3. *Guidelines Specifically for Row Houses.* In addition to all the guidelines noted in this document, Row Houses must:

- a. Not exceed three floors.
 - b. Not exceed a maximum of six units in length.
 - c. Be modulated horizontally and vertically; long flat facades are not allowed; monolithic or overly repetitive forms are to be avoided.
 - d. Have varying rooflines.
-



Multi-Family Stacked Housing
Example

4. *Guidelines Specifically for Multi-Family Stacked Flat Housing.* In addition to all the guidelines noted in this document, Multi-Family Housing must:

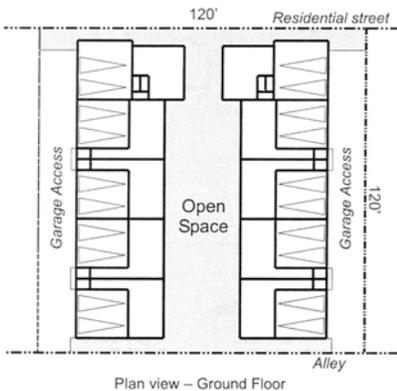
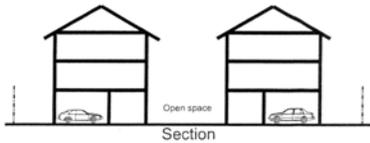
- a. Be designed in general compliance with City of Blaine Multi-Family Design Standards. Where the City of Blaine Standards and standards given in this document conflict, those in this document shall be given precedence.
- b. Buildings shall have five floors maximum over parking structure.

c. Buildings must be modulated horizontally and vertically; long flat façades are not allowed. Monolithic or overly repetitive forms to be avoided.

d. The scale of these buildings is to be integrated into the surrounding neighborhood scale of row houses.

e. Roof lines must vary; roofs are to be pitched, particularly on lower roofs, to blend in with the scale of adjacent buildings; large areas of blank roof must be broken up with smaller features such as dormers.

f. At grade entrances for lower floor units is preferred so that they are similar to town homes; lobby area must have an at grade entrance and face the street; porches and stoops are recommended for at grade entrances.



Parking Under the Building

g. Parking is preferred under the building; if additional parking is required, it must be located in a manner minimizing visibility from the street.

h. Garage entrances and driveways must be located so as not to interfere with the pedestrian environment; these should be located away from street façades wherever possible.

i. Trash enclosures must be located on site and screened from public view

5. Relationship to the Street.

- a. Provide a separate walkway from the street to the home entrance.
 - b. Driveways cannot be used for residential entry.
 - c. Residential entrances must be oriented towards the street and be distinctly visible from the street.
 - d. Windows to living spaces must be placed on the front façade.
-

7. *Carriage House Accessory Dwelling Units.* All residential units are permitted to have one accessory dwelling unit (ADU) per primary dwelling unit over a detached garage provided that:

- a. Carriage units are architecturally designed in a manner consistent with the primary house.
 - b. Setbacks are the same as those required for detached garages.
 - c. All architectural design guidelines set forth in this document are met.
-

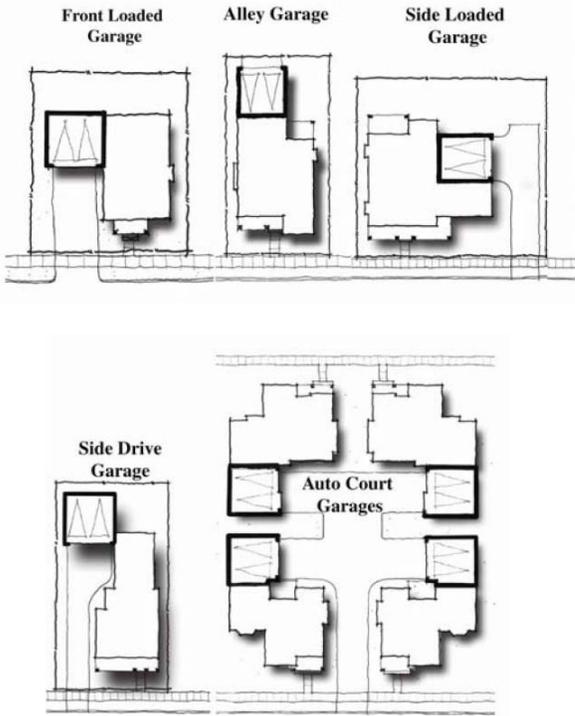


Carriage House Accessory Dwelling
Unit Example

6. *Garages and Driveways.* The design objective of this section is to minimize the visual impacts of the garage through the use of alleyways, recessed garage doors (front loaded), and the emphasis of the porch and front door.

a. Standards.

(1) On-Site Garage.



Acceptable Garage Locations

(a) On-site garages shall include both attached and detached structures.

(b) Garages must be setback a minimum of 10 ft from the front facade of the house, not including the porch, and the total driveway length to be no less than 18 ft as measured from the face of the garage to the back of sidewalk.

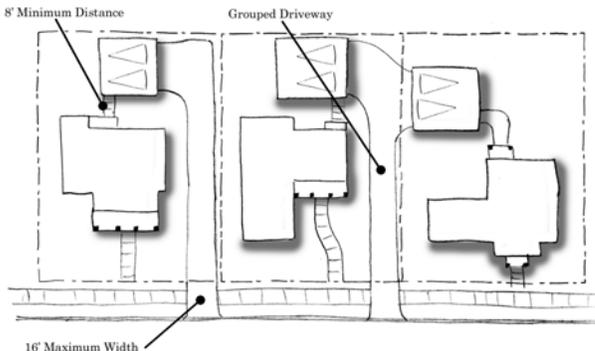
(c) Detached garages shall maintain an 8-foot separation from any dwellings, including the on-site dwelling served by the garage. Not applicable to carriage house dwelling units.

(d) For duplex, cottage single family and row houses, alley loaded garages must be provided where feasible so garages are not visible from the street.

(e) For Single Family Detached Homes:

(i) Garages that are oriented 90 degrees to the street, so that garage doors are not prominent along the streetscape, may reduce the garage front yard setback to 10'.

(ii) Garages must not have long blank walls facing the street and should have windows facing the street.



Single Family Home Detached Garage

(iii) Driveways should not be wider than the garages they serve; driveways should be a maximum 16 ft wide at the property line.

(iv) Grouping of driveways together to minimize curb cuts is preferred.

(v) Vary location of the garages so that they are not always on the same side of the house along the streetscape.

(vi) Garages may be detached or attached. If detached, they should be located at least a minimum 5 feet back of the front line of the house, or preferably, behind the house.

(f) Detached Garages may include one Accessory Dwelling Unit (ADU) per primary housing unit.

(2) Shared Detached Garages.

(a) Shared detached garages are allowed and can be used to meet residential parking requirements.

(b) Each housing unit shall be assigned a garage space and may share the structure with other homes.

(c) Shared detached garages shall be located within a 300 foot walking distance from the unit to which the garage is assigned.

(d) Shared detached garages shall not exceed 44 feet in width and shall maintain an 8-foot separation from

any dwellings. Not applicable to carriage house dwelling units.

(e) Shared detached garages for Residential Single Family units may include one Accessory Dwelling Unit (ADU) per primary housing unit.

(3) Garage Design.

(a) All garages shall follow an architectural style similar to the homes.

(b) If sides are visible from streets, lanes, sidewalks, pathways, trails, or other homes, architectural details such as windows, eaves, trellises, arbors or extra trim treatments, shall be incorporated in the design to minimize the impacts of the façade.

(4) Carports. Carports are prohibited.

b. Guidelines.

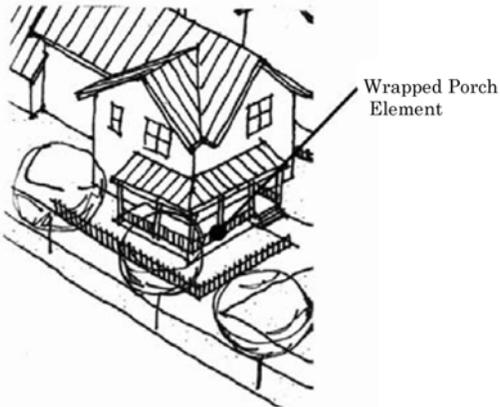
(1) Avoid garage doors at the end of view corridors.

(2) Lots that take access directly from a neighborhood street shall require a layout that lessens the visual impact of the garage doors.

(3) Garages shall not be the dominant visual element in any development.

7. *Privacy.*

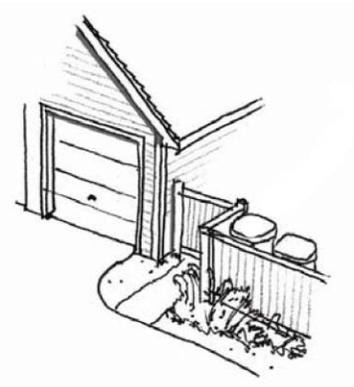
- a. Side wall windows should be arranged as not to align with sidewall windows of adjacent homes.
-



Corner Lot Detail

8. *Corner Lots.*

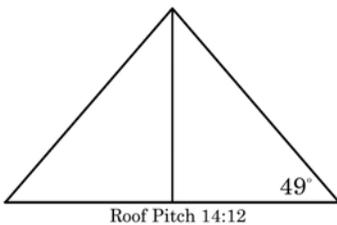
- a. Each façade facing the public realm must be treated as a front façade including the architectural standards noted under that section.
 - b. These lots must have at least two façades, and sometimes three (including the rear façade) facing the public realm.
 - c. Front porches are encouraged to wrap around corners on corner lots.
-



Acceptable Garbage Location

9. *Trash and Recycling.*

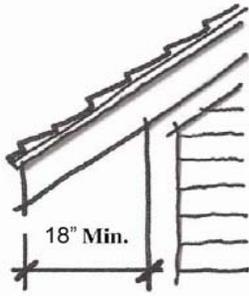
- a. Containers are to be stored within private garages or where screened from public view and brought to the street on collection day only.
-



Roof Pitch Limits

10. *Roof Pitches.*

- a. Must be between 6:12 and 14:12 for primary roofs.
- b. Roof overhangs are to be consistent with the Northwest Craftsman style of home and a minimum of



Roof Overhang Minimum



Dormer

18 inches. Overhangs must be consistent around a home.

c. Large areas of roof exposed to the street must be broken up with dormers.

d. Varied roof profiles along the street are highly encouraged.

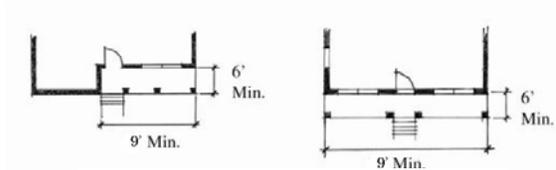
11. Porches and Stoops.

a. Porches must be a minimum of 9 feet wide by 6 feet deep.

b. Stoops must be a minimum of 6' wide by 6 feet deep.

c. Both must provide weather protection above the front door.

d. Porches must have railings and be a minimum of 18 inches above grade.



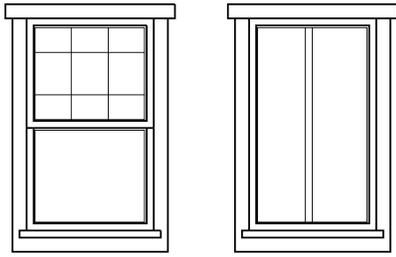
Minimum Porch Size



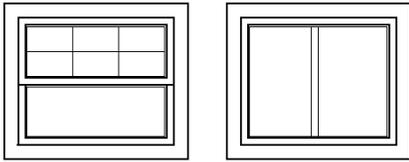
Minimum Stoop Size

12. Windows and Entry Doors.

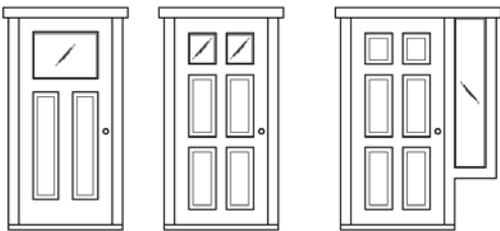
a. Windows are to be vertically proportioned; horizontal sliding windows must also be vertically proportioned.



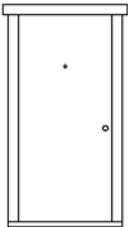
Acceptable: Vertically Proportioned Windows



Unacceptable: Horizontally Proportioned Windows



Acceptable Entry Doors



Unacceptable Entry Door

b. Accent windows are encouraged and do not need to meet the vertically proportioned criteria. Triangular shaped windows are not allowed.

c. Windows on primary or street facing façades must have divided lights with mullions and muntin bars.

d. All windows, on all façades, must be trimmed with a minimum 1 by 4 inch trim with 1 inch widow sill.

e. All facades should have windows. Blank walls are to be avoided and shall be permitted only in rare circumstances where dictated by reciprocal use easements and or privacy.

f. Entry doors are to be consistent with the style of the home and are to be multi-paneled and should either have a window feature or side light.

13. *Materials for Siding and Roofs.*

a. At least two siding materials must be used on any single building.

b. Siding materials should be comprised of pebbled dashed stucco, brick, stone, wood lap siding, and or cut wood shingles.

c. Artificial or imitation stone panels are not allowed, however manufactured stone veneers are acceptable

d. Plywood siding, vinyl siding, or T111 is not allowed.



Horizontal Trim Break

e. Materials must not change at outside corners; transitions in materials must occur at inside corners or with appropriate horizontal trim breaks, such as belly bands, between materials.

f. Belly bands, fascia boards and water table boards are strongly encouraged when consistent with the style of the home; when used they must be consistently applied to every façade of the home.

g. Roofing material should be consistent with the style of home and be comprised of asphalt composite shingles, cedar shake shingles or non-reflective metal.

h. Reflective roofing material is not allowed.

14. Colors.

a. A diverse palette of colors is encouraged; uniform color palettes are not allowed.

b. Color should be used to differentiate building materials and massing; color changes unrelated to building form or materials are not allowed.

c. Roof colors should not be the same shade for every building.

d. Roof colors should be warm or cool grey, charcoal grey or brownish colors and should harmonize with the color of the home.

e. Window trims, accent trims and bellybands must be painted a contrasting color.

f. Entry doors must be painted a contrasting, accent color.

g. Metals must be powder coated.

15. Definitions

Northwest Craftsman: An architectural style derived from the American Arts and Crafts movement featuring straightforward expression of the structure of the dwelling with exposed rafters, square columns and beams, flat, square-edged trim elements in ways that enhance the frame and structure of the house.

Cottage Single Family: Smaller Single Family detached homes on lots less than 4000 square feet with alley loaded garages. Homes are generally less than 1800 square feet.

Attached Single Family Dwelling Units: Two units side by side with one shared wall/zero lot line that is sold fee simple. Lots are at or above approx. 3750 square feet. On the Approved Site Plan, they have alley-loaded garages.

Row house: Three to 6 units attached in a row sold as zero lot line/fee simple. Middle units have a front and rear yard only with no side yard; end units have a side yard. Lots in the middle will be smaller, size depends on width of unit- generally all lots are at or under 2000 square feet. In this plan, they have alley loaded garages and this is preferred and should be required of row houses, otherwise garages dominate the streetscape. Special regulations shall include no minimum lot size, no public street/utility frontage is required for each lot,

reduced yard setbacks permitted, no maximum lot coverage requirement and internal street standards may be reduced.

Live/Work Row House: As above with a front/street side flex space at grade that can be used as work space or home business for the owner. Types of work spaces: accountant, massage therapist, personal trainer, artist or photographer, architect, hair dresser, etc. Only those businesses that guarantee minimal traffic and are permissible under the Blain Municipal Code "Home Occupation" regulations shall be allowed.

Stacked Flats: Units are on one floor, different units stacked one on top of each other, usually arranged around a central corridor; can only be sold as condominium because ownership is multiple from ground to roof; no private land associated with it; can also be rentals.

Front: The primary entry of a building and the façade presented to the public.

7.3 SIGNAGE STANDARDS

1. Project Signs. One Residential Subdivision Identification sign, which may be indirectly lighted, may be located near each main entrance road on private property. Each sign shall not exceed 16 square feet in area, be limited to a maximum height of 6 feet and setback no less than 10 feet from public right-of-way. The message shall be limited to the name logo of the project only. An additional area of 50% of the allowed signage area may be added as a decorative background provided that it does not contain any lettering or logos.

2. Business Signs. Within those portions of the Property where commercial uses are allowed, signs for each commercial activity shall adhere to the design guidelines, scale and location requirements set forth by City of Blaine Sign Ordinance Supplement.

3. Home Occupation Signs. These signs shall be limited to 2 square feet in size, shall be compatible with the home design and color, and shall not be lighted.

7.4 LANDSCAPING STANDARDS

1. Design Objective. To enhance the visual appearance of the neighborhood, provide a unifying theme, to preserve the natural wooded character of the Pacific Northwest, to promote utilization of natural systems, and to reduce the impacts on storm drainage systems and water resources. The following figure shows the location of the landscaping requirements.



Figure 11 – Landscaping Requirements Key Map

a. Standards.

(1) Planting Calculation and Installation.

(a) Existing trees may be used to meet the tree requirements in the planting calculations. See section associated with appropriate community plan for existing tree credit. Noxious weeds are not permitted.

(b) Place all shrubs and perennial plants in beds mulched with finely ground bark. The bark must cover the entire planting bed to a depth of 2 inches.

(c) Install a mulch ring at the base of each canopy and ornamental tree. At time of planting, the ring must have at least a 2-foot radius, measure from the center of the tree trunk. This mulch ring must be of organic material and be a depth of 2 inches.

(2) Neighborhood Parks. Neighborhood Parks shall be large tracts of contiguous land that serve as social and recreational focal points for the surrounding neighborhoods and shall either be designed as formal parks with active and passive recreational spaces or as natural areas with trails and nature interpretive amenities.



(a) One 2-1/2 inch caliper canopy or ornamental tree shall be planted for every 1,000 square feet.

(b) Shade trees shall be provided adjacent to play structures and at other elements in the park, such as sport courts and benches.

(c) A pathway, with a minimum width of 3 feet, shall connect a park to neighborhood streets, access lanes, or other pedestrian connections.

(3) Common Greens. Common greens shall be shared, maintained grassy open areas typically adjacent to residential lots.

(a) One 2-1/2 inch caliper canopy or ornamental tree shall be planted for every 1,000 square feet.



(b) The common greens shall be planted with plants that reflect the architectural character and the intended use of the greens.

(c) Sidewalks or pathways are encouraged at the edge of the green to allow a larger usable green and easy access to the homes.

(d) A pathway, with a minimum width of 3 feet, shall connect a common green to neighborhood roads, access lanes, or other pedestrian routes.

(e) The minimum lawn coverage of a common green area shall be 70 percent.

(4) Pocket Parks. Pocket parks shall be small parcels of maintained open grassy areas designed with passive recreational activities such as, public



art, monuments, and or casual seating.

(a) One 2-1/2 inch caliper canopy or ornamental tree shall be planted for every 1,000 square feet.

(b) The pocket parks shall be planted with plants that reflect the architectural character and the intended use of the pocket parks.

(c) Sidewalks or pathways are encouraged at the edge of the pocket park to allow a larger usable park and easy access to the homes.

(d) A pathway, with a minimum width of 3 feet, shall connect a pocket park to neighborhood streets, access lanes, or other pedestrian routes.

(e) The minimum lawn coverage of a pocket park shall be 70 percent.

(5) Street Trees on Neighborhood Streets. Streetscapes shall be designed to provide a unifying theme that enhances the visual appearance of the neighborhood.

(a) Street trees are required on all neighborhood streets to provide shade and to calm traffic.



(i) Trees shall be a minimum of 2-1/2 inch caliper.

(ii) Trees must be spaced at 30 – 40 feet intervals on center.

(iii) Trees shall be placed so as not to block sight distance or create a safety concern.

(iv) Tree species shall be approved by the City.

(b) Ground cover or perennials must fully cover the remaining landscape area.

(6) Street Trees on Access Lanes. Streetscapes shall be designed to provide a unifying theme that enhances the visual appearance of the neighborhood.

(a) Street trees are required on all access lanes to provide shade and to calm traffic.



(i) Trees shall be a minimum of 2-1/2 inch caliper.

(ii) Trees must be spaced at 30 – 40 feet intervals on center and

set back approximately 4 feet from back of curb or roadway edge. Trees may be planted on a private lot, but should be located in the right-of-way where feasible.

(iii) Trees shall be placed so as not to block sight distance or create a safety concern.

(iv) Tree species shall be approved by the City.

(7) Pedestrian Easement.

(a) Trees are required along all pedestrian easements to provide shade.

(i) Trees shall be a minimum of 2-1/2 inch caliper.

(ii) Trees must be spaced at 30 – 40 feet intervals on center.

(iii) Trees shall be placed so as not to block sight distance or create a safety concern.

(iv) Tree species shall be approved by the City.

(b) Shrubs are required for 15 percent of easement space.

(i) Shrubs shall be planted a maximum 36 inches on center.

(ii) Shrubs shall be a minimum 2-gallon for native shrubs and 3-gallon for non-native.

(8) Front Yards. Landscaping of front yards shall be designed in a manner consistent with the unifying themes of the neighborhood.

(a) Entry walks shall have a minimum width of 3 feet and a maximum width of 4 feet.

(b) One tree having a minimum caliper size of 2 inches or height of 8 feet shall be planted in the front yard of each unit that has a front yard setback of 15 feet or greater. This tree is additional to any tree required as a street tree.

(c) Shrubs.

(i) A continuous row of shrubs having a maximum spacing of 3 feet on center shall be planted adjacent to that portion of a foundation facing a public space.

(ii) Shrubs shall be a minimum 2-gallon for native shrubs and 3-gallon for non-native.

(9) Side Yard Along Public Space. Planting shall be required along fences that face a street or public space.

(a) Shrubs.

(i) A continuous row of shrubs having a maximum spacing of 3 feet on center shall be planted adjacent to that portion of a fence or foundation facing a public space.



(ii) Shrubs shall be a minimum 2-gallon for native shrubs and 3-gallon for non-native.

(b) Planting is required along fences that face a street. Shrubs, trees, and vines shall be used to soften the fence's public side.

(10) Rear Yard.

(a) One tree having a minimum caliper size of 2 inches or height of 8 feet shall be planted a minimum of 5 feet from the alleyway pavement edge.

(b) Planting is required along fences that face a street. Shrubs, trees, and vines shall be used to soften the fence's public side.

(c) Shrubs.

(i) A continuous row of shrubs having a maximum spacing of 3 feet on center shall be planted adjacent to that portion of a foundation facing a public space.

(ii) Shrubs shall be a minimum 2-gallon for native shrubs and 3-gallon for non-native.

(11) Alleyway Plantings.



(a) Space between the alleyway and fence must be landscaped with native and drought-tolerant shrubs, ground cover, and upright trees where space and layout allow.

(12) Landscaping Utility Areas.

(a) To minimize their appearance, transformers and other utilities shall be landscaped with a 3-foot high, continuous vegetated screen, except in areas where access is required. Plants shall be evergreen.

(13) Natural Open Space

(a) Areas designated as “Natural Open Space” shall consist of undeveloped tracts of land and shall emphasize retention of existing native vegetation and mature trees where possible enhanced with plantings of native species of drought-resistant plants and trees along streets, in park areas, and around stormwater ponds where necessary. Areas may be access through the use of pedestrian trails.



(14) Buffer Areas

(a) Areas designated as “Buffers” shall consist of retained and enhanced native vegetation providing screening and a natural transition to adjacent land uses.

(b) Guidelines.

(1) In general and where feasible, plant preservation should take precedence over transplanting, transplanting over planting new, and planting native over ornamental species.

(2) Existing topsoil should be used where feasible. The clearing and site preparation of larger development areas should save and stockpile existing topsoil for plant adaptation.

(3) The use of native drought-tolerant plants is encouraged.

7.5 FENCE AND HEDGE STANDARDS

1. Design Objective. The incorporation of fences and hedges around a housing unit to define private spaces.

a. Standards. Fences and hedges shall not be placed near neighborhood streets, access lanes, or alleyways in such a way to create a safety or entering sight distance concern.

(1) Front Yard Decorative Fence. If used, front yard fences shall be decorative and help to define semi-private areas in the front of the home.

(a) The maximum height shall be 36 inches

(b) Front yard decorative fences shall be located a minimum of 1 foot from parcel line to allow for planting between edge of sidewalk or right-of-way and fence.

(c) Front yard decorative fences shall provide a balance of solid surfaces and voids, such as picket or Kentucky rail fence styles.

(d) Front yard decorative fences shall be constructed of wood, simulated wood, iron, or masonry. Solid fences and chain link shall be prohibited.

(2) Hedges. If used, hedges shall be continuous along the front and side property line, and street frontage.

(a) The maximum height of a hedge in a front yard shall be 36 inches.

(b) Hedges located in a rear yard do not have a maximum height standard.

(c) The maximum height of a hedge for the first 15 feet of a side yard on a corner shall be 36 inches. There is no maximum height for the remaining side yard.

(d) Hedges located along an interior side yard shall be prohibited.

(e) Hedges are not allowed if a side yard use easement is used.

(f) Evergreen native plant material is preferred for year-round coverage.

(3) Privacy Fencing. If used, privacy fencing shall only be permitted on back, side and portions of corner side yards and shall be in character with the home's architecture.

(a) The maximum height of privacy fencing in a rear yard shall be 6 feet.

(b) Privacy fencing in a front yard shall not be permitted.

(c) For corner lots where the side yard privacy fencing would be placed facing the street or access lane, the maximum height of the first 15 feet of the fencing as measured from the front façade, shall be 3 feet. The maximum height of the remainder of the privacy fencing shall be 6 feet.

(d) Fences are prohibited along the interior side yards.

(e) Privacy fencing adjacent to a public space shall be setback a minimum of 1 foot from the property line.

(f) If the privacy fencing is located along the alley way, a gate must be provided for access to the alleyway.

(g) Privacy fencing shall be constructed of wood, simulated wood, iron, or masonry. Chain link fencing shall not be permitted.

8 Open Space

8.1 ACTIVE AND PASSIVE OPEN SPACES

The project design includes 23.6-acres of active and passive open space areas, which comprise approximately 20% of the project site gross area. The open spaces will include accessible and open park-like areas, incorporated

existing vegetation and new landscape planning, and integrating the proposed stormwater ponds as an aesthetic feature. The open space areas will be created as separate tracts within each phase, to be owned by an East Maple Ridge Homeowner' Association. The open space tracts are delineated on the Preliminary Plat and Planned Unit Development Site Plans included in the development application.

8.2 TRAILS

The street and trail systems are designed to facilitate pedestrian connections within and through the overall site plan. The project will incorporate a multi-use



trail corridor along the Motts Hill Parkway street alignment, which is designed to integrate with the transportation element of the City's East Blaine Sub-Area Comprehensive Plan; as such the trail will connect in the future with other City parks and the

downtown business areas. Several north/south multi-use trail connections are provided in the site plan, linking bicyclists and pedestrians to H Street and to active and passive open space areas within the PUD.

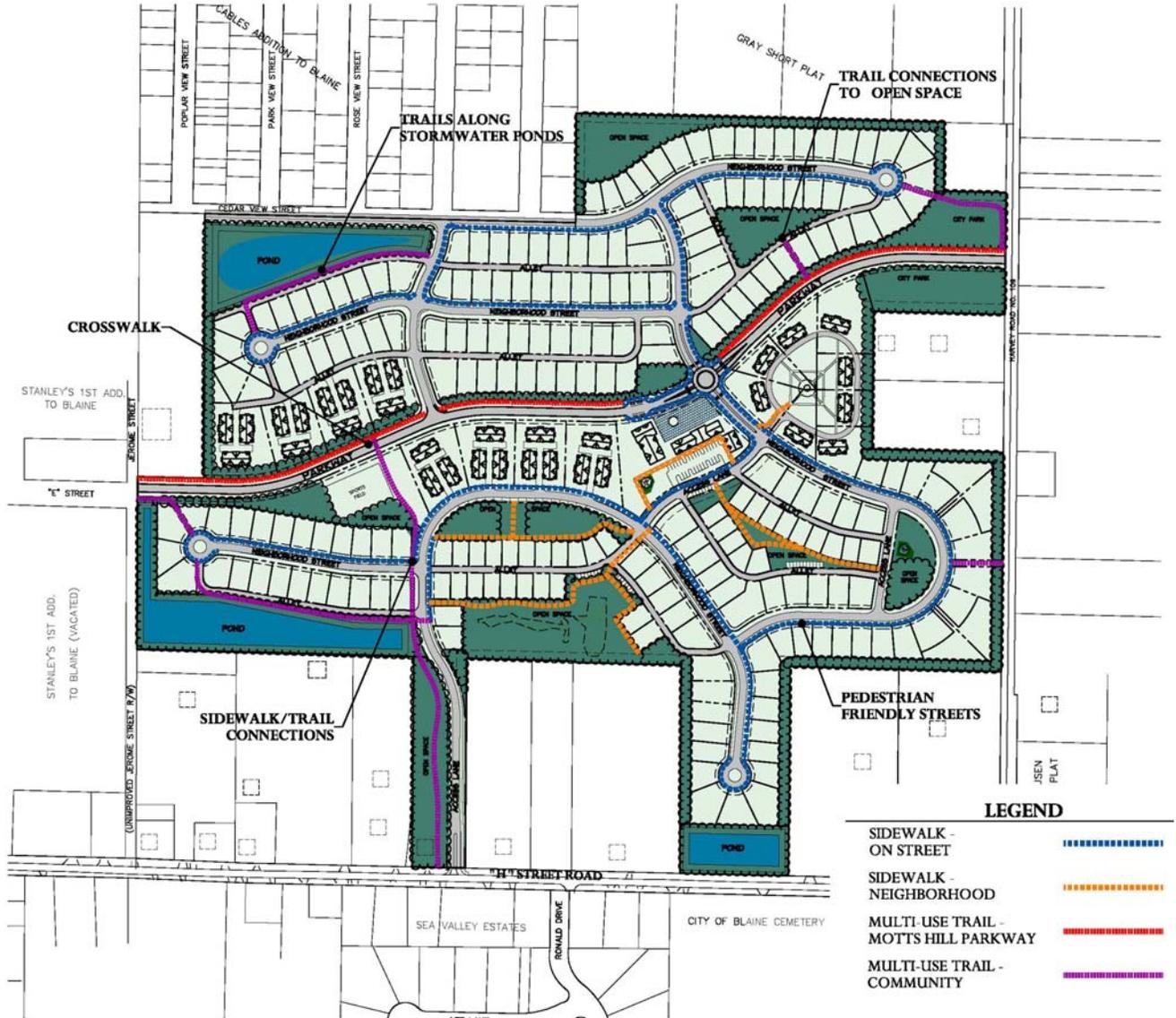


Figure 12 - Pedestrian Circulation System

a. Standards.

(1) Multi-Use Trails: The proposed Community multi-use trails and Motts Hill Parkway multi-use trails will be 8-feet in width with 2-foot shoulders, with a crushed limestone surface treatment. The intent of these trails is to provide accommodation for two-way bicycle traffic as well as pedestrians. No lighting is required for these trails; however, some lighting may be desirable at

intersections and points of interest along the trails, which shall be coordinated with street lighting in the final design for each phase. The trail system will be maintained by the East Maple Ridge Homeowner's Association, except for the portions within the City right-of-way, along Motts Hill Parkway, which will be maintained by the City as part of the planned regional trail system.

(2) Sidewalks – On Street The proposed on street sidewalks shall be 5-feet in width and constructed of concrete per City of Blaine standards. The intent of the on street sidewalks is to provide pedestrian friendly environment along the street system of the development. The on street sidewalk system will be maintained by the City of Blaine.

(3) Sidewalks – Neighborhood Connection The proposed neighborhood connection sidewalks shall be 3-feet in width and constructed of concrete per City of Blaine standards. The intent of the neighborhood connection side is to provide pedestrian access to residential and commercial uses that do not front directly on neighborhood streets and access lanes. The neighborhood connection sidewalks shall be maintained by the East Maple Ridge Homeowner's Association.

8.3 STORMWATER PONDS

The stormwater pond facilities are an important component of the open space design for this project, and will be integrated into the open space areas, as illustrated on the site plans. Specific design factors must be considered to ensure the aesthetic integration of the ponds as desirable features in the landscape. Such factors will include:

- Ponds are located where possible adjacent to other open space features, such as footpaths and park areas.
- Pond slopes are minimized along pedestrian trails and abutting lots and other open space areas to reduce the need for fencing around the ponds, allowing the ponds to be visually and physically connected to the open space. 3:1 slopes will be required whenever possible with

possible exceptions allowed to accommodate existing grade or pedestrian access conflicts.

- Open space area adjacent ponds, above the top of pond slope will be landscaped as Park Areas, as defined in the Project Development Standards (Section 7.4). Pond slopes will be seeded and planted in accordance with the Department of Ecology standards and guidelines for maintenance and water quality treatment benefits.
- Benches and picnic tables will be provided where appropriate to increase the use of the ponds as an attractive feature of the landscape.
- Design factors for aesthetics and open space use will be included in the final construction plans and details for construction of each phase.
- Ponds will be built with naturalized shape and contours as the space and design allows.

8.4 LANDSCAPING

Landscaping of the project open spaces shall be in accordance with Section 7.4 of this Guidebook and emphasize retention of existing native vegetation and mature trees where possible, and use of native drought-resistant plants and trees along streets, in park areas, and around stormwater ponds. Final landscaping plans shall be submitted to the City with the construction documents for each phase. The landscaping plans shall be generally consistent with the Preliminary Landscaping Plan (Figure 9).

8.5 MAINTENANCE

Maintenance of all open space areas and the private storm drainage ponds on the site will be carried out by the East Maple Ridge Plat Homeowner's Association (HOA). Specific responsibilities and minimum requirements will be included in the HOA agreement and covenants and restrictions documents, to be reviewed by the City and recorded prior to finalization of the first phase of the Plat.

9 Utility Servicing Plan

9.1 WATER

The project will be served by the City water system, from the existing 12-inch water main currently located on the site. Based on the Public Works Department comments, the City currently has adequate water supply to serve this project. Water is available on a first come first serve basis. In 2007, the City had 1.21 million gallons per day available water (short term) and further work is underway by the City to increase the water supply capacity for the future.



Figure 13 – Existing Water System

The development will include new 8-inch water mains looped through the site within the roadway areas, to provide potable water service and fire protection throughout the project. Fire hydrants will be installed per City and Fire District standards for spacing and placement in residential areas. The preliminary

utility plans (Section 12) illustrate generally how the project will extend water service mains on-site to serve the proposed lots.

9.2 SANITARY SEWER

The project will be served by the City sewer system. The applicant has proposed to extend the existing sewer main on H Street to the project, and has committed to future improvements to increase the sewer conveyance capacity available. Discussions with Public Works indicate that the existing sewer system has adequate capacity to serve East Maple Ridge, with developer extension of a sewer main connection to the project, which will be sized adequately for future service of other property based on the East Blaine Sub-area Infrastructure Plan.



Figure 14 - Proposed Sewer Extension

9.3 ELECTRICAL SYSTEM

A City electrical system will be extended throughout the site to provide service meeting City standards.

9.4 STORM DRAINAGE

Stormwater runoff from the proposed development will be detained and treated on-site to limit potential downstream impacts, based on City of Blaine code and the 2005 Department of Ecology Stormwater Management Manual for Western Washington (SWMM). A Preliminary Stormwater Site Plan (SSP) has been prepared by APC Engineers for this development, the report is attached as Appendix C.

The topography of the site, and existing drainage system patterns dictate that the project will require multiple stormwater facilities. The Preliminary SSP proposes to construct three combined detention and water quality pond facilities, located in the lowest topographic areas on the site.

Water quality treatment of runoff from the site will be provided in created stormwater treatment wetlands within each detention pond. Stormwater treatment wetlands provide effective treatment of runoff meeting state standards for water quality, utilizing mechanical dissipation and uptake of pollutants by the aquatic vegetation and associated microbiological community.

Additionally, implementation of an effective Erosion Control Plan will be required for each phase of construction, to protect pond facilities and the downstream drainage system.

9.5 PRIVATE UTILITIES

Private utility services including telephone, cable television, and natural gas, will be extended into the project by the local purveyors serving the area. Private utility lines through the project will be located primarily in a 10-foot wide easement along each side of the street rights-of-way.

10 Public Streets

10.1 TRAFFIC IMPACT STUDY

The City's Sub-Area Comprehensive Plan currently in progress for the East Blaine area includes a plan for a secondary collector Parkway north of and parallel with the H Street. Since 2004, the project team has worked closely with staff and outside consultants in correspondence and meetings to accommodate the projected traffic impacts in East Blaine. This design effort provides for a Parkway Street alignment that runs east and west through the center of the site, connecting Harvey with D Street. The PUD proposes a round-about intersection near the commercial/civic center, as shown on the Site Plan, thereby allowing for efficient pass-through traffic from an east west direction. The street plan incorporated into the site design additionally provides for controlled circulation within the plat as well as minimized connections to H Street. The Caples Addition neighborhood to the north has been tied to the street system to allow the residents to gain a second means of ingress and egress. The internal plat street layout and proposed street sections are designed to provide traffic calming and avoidance of "cut-through" traffic, while maintaining an efficient circulation system.

The attached *East Maple Ridge Traffic Report* prepared by Gibson Traffic Consultants analyzes the existing traffic conditions and the impacts from this PUD / plat on the traffic system. The report concludes that the development will not impact any intersections or arterials operating at deficient levels of service LOS and will not *cause any* intersections or arterials to operate at deficient levels of service.

10.2 OFF-SITE STREET IMPROVEMENTS

The project proposed to construct off-site three-quarter street improvements to the abutting section of Cedar View Street along the north margin of the property. This improvement will be constructed in Phase 6, as outlined in the project phasing plan section of this document. The improvements will include a 22-foot paved street, with a curb and sidewalk along the south side, and

connection points to the abutting roads in Caples Addition. This improvement requires dedication of public right-of-way along the north property line, as shown on the Preliminary Plat. A future right-of-way dedication and street improvement by others will be needed to connect Cedar View Street west through to Jerome Street.

10.3 INTERNAL STREET SYSTEM

The internal street system for the East Maple Ridge Development will include small, pedestrian friendly neighborhood access streets, as well as incorporating a segment of the planned Motts Hill Parkway roughly through the center of the site.

Motts Hill Parkway is planned as an arterial street to serve as a vital transportation link for the East Blaine Sub-area to downtown Blaine, Interstate 5, and the international border. The segment of the Parkway designed through East Maple Ridge will run from the E Street right-of-way on the west, through to Harvey Road to align with the City's existing water main easement. This alignment and the configuration and proposed section of Motts Hill Parkway through East Maple Ridge were planned jointly with the project proponent and the City Public Works and Community Development departments, in a series of meetings and discussions addressing the capacity, alignment needs, and aesthetics of the proposed Parkway. In addition to functioning as an arterial roadway, Motts Hill Parkway is planned to include a major multi-modal pathway setback from the street, to provide recreational and alternative transportation opportunities and connections throughout East Blaine.

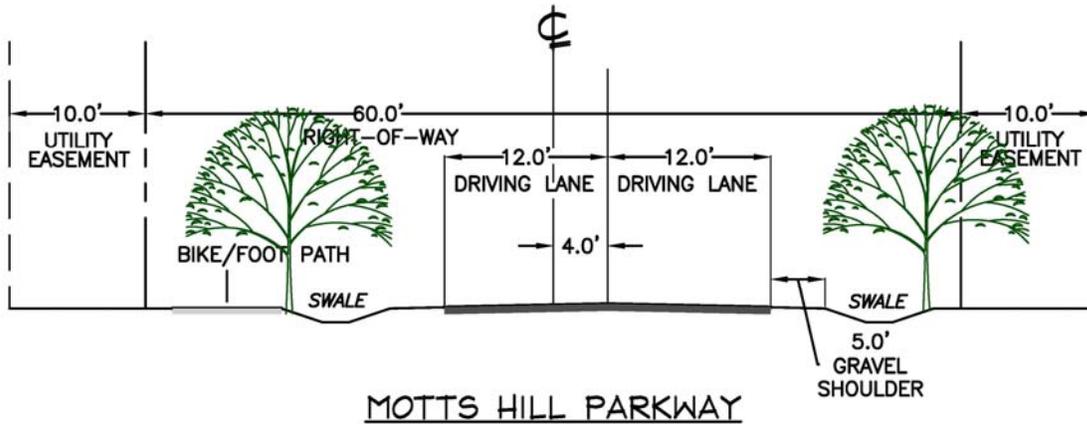
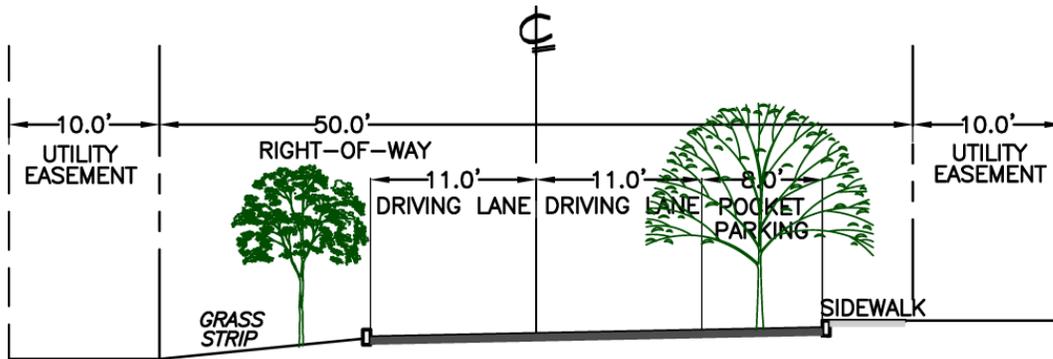


Figure 15 – Motts Hill Parkway Typical Section

The development plan proposes only limited street connections to Motts Hill Parkway, with one main connection each for the northern and southern halves of the property, as well as an additional connection to provide through access to the existing Caples Addition neighborhood to the north. Direct driveway access to Motts Hill Parkway is prohibited.

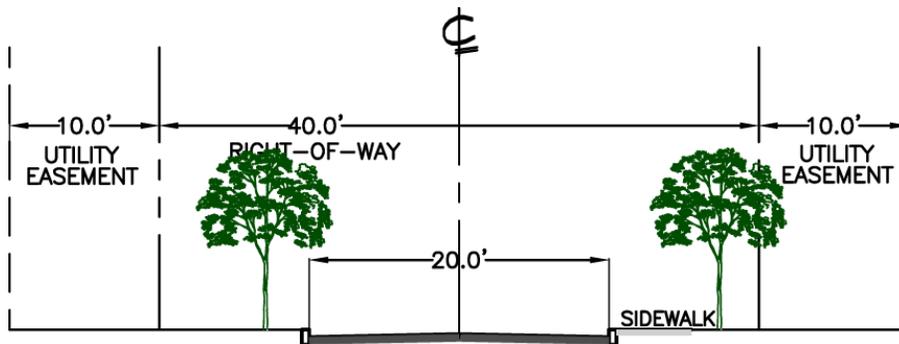
The planned Neighborhood Street sections will provide for on-street parking while maintaining a pedestrian-friendly street, with limited driveway access allowed where needed. The objective is to provide a neighborhood atmosphere that emphasizes the streetscape and encourages traffic calming.



NEIGHBORHOOD STREET

Figure 16 - Neighborhood Street Typical Section

Areas of the site where on-street parking is not needed or desirable, but through access is required will utilize Access Lane street sections, which maintain a pedestrian character, and a narrowed pavement section to discourage speeding and “cut-through” traffic.



ACCESS LANE

Figure 17 – Access Lane Typical Section

An important component of the site and street layout design is the incorporation of Alleys for access to many of the residential lots. Use of alleys for garage access improves the appearance and quality of the street system by allowing home design to focus on the front porch and neighborhood character without

garages dominating the streetscape. Alleys also greatly increase the pedestrian friendliness of the street sidewalk network by reducing driveway cuts and pedestrian conflicts with vehicles.

The proposed lot design and architectural standards (Section 7 of this Guidebook) require use of alleys for access where possible, and limit the allowed placement of garages relative to the building and the street.

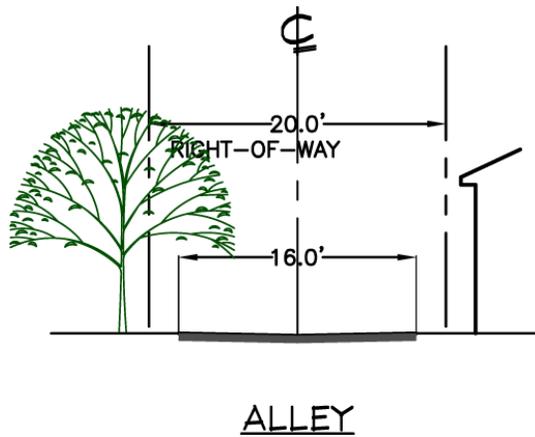


Figure 18 – Alley Typical Section

11 Fiscal Impact Analysis

12 Project Plans

List of Attached Drawing Plan Sheets:

- (1) PUD Site Plan
- (2) Preliminary Plat, 1 of 3
- (3) Preliminary Plat, 2 of 3
- (4) Preliminary Plat, 3 of 3
- (5) Project Phasing Plan
- (6) Existing Site Conditions Map
- (7) Preliminary Utility Plans
- (8) Conceptual Street Lighting Plan
- (9) Proposed Street Sections, 1 of 2
- (10) Proposed Street Sections, 2 of 2
- (11) Pedestrian Circulation Plan
- (12) Building Lot Configurations
- (13) Landscape Plan Concept

13 Appendix A: SEPA Environmental Checklist

14 Appendix B: Wetland Delineation Report

15 Appendix C: Preliminary Stormwater Site Plan

16 Appendix D: Traffic Analysis

17 Appendix E: Geotechnical Report

18 Appendix F: Economic Study

19 Appendix G: Amended Development Agreement

20 Title Report
